

**01-21-19 Posting Date**  
**LUC-York Street**  
**PID No. N/A**  
**City of Oregon**  
**Response Due Date: 02-08-19**

**Communications Restrictions**

**Please note the following policy concerning communication between Consultants and the City of Oregon during the announcement and selection process:**

During the time period between advertisement and the announcement of final consultant selection, communication with consultants (or their agents) shall be limited as follows:

**Communications which are strictly prohibited:**

Any discussions or marketing activities related to this specific project.

**Allowable communications include:**

Technical or scope of services questions specific to the project or RFP requirements. Specific questions concerning the project details can be emailed to [rshultz@ci.oregon.oh.us](mailto:rshultz@ci.oregon.oh.us).

**Project Description**

The City of Oregon is requesting Letters of Interest (LoI) from consultants for planning and engineering services. The services include preparation of construction contract plans and specifications for the replacement of two existing bridge structures within the City of Oregon in Lucas County. The bridges are the York Street Bridge over Amolsch Ditch (SFN 4863232) and the Norfolk Southern Railroad Bridge over Amolsch Ditch, which is located approximately 190 ft south of York Street.

The existing York Street Bridge over Amolsch Ditch (SFN 4863232) consist of twin 54" reinforced concrete pipes with stone headwalls. The Norfolk Southern Railroad Bridge over Amolsch Ditch consist of twin 54" corrugated metal pipes with no head walls.

The city anticipates that the proposed York Street structure will be an approximately 12 ft span 4-sided structure based on hydraulic information and recent bridge replacement projects for the area. The profile and alignment of the existing roadway is not anticipated to be modified by the project. The proposed York Street structure will need to be designed to accommodate two 12-foot wide lanes and 5 ft graded shoulders along with safety grading so as to not require guardrail over the structure. Some minor shoulder work to the approach roadway may be necessary to accommodate safety grading.

The city anticipates that the proposed Norfolk Southern Railroad structure will be an approximately 12 ft span 4-sided structure based on hydraulic information and recent bridge replacement projects for the area. The profile and alignment of the existing railroad is not anticipated to be modified by the project. The proposed Norfolk Southern Railroad structure will need to be designed to meet the requirements of Norfolk Southern.

Traffic is to be detoured during construction.

This project is planned to be funded with local funds. Construction funding has been included the City of Oregon's 2019 CIP Budget.

**Estimated Construction Cost:** N/A

**Prequalification Requirements**

Prequalification requirements for this agreement are listed below. All prequalification categories may be met by the prime consultant or a subconsultant.

DESIGN SERVICES:

Non-Complex Roadway Design;  
Limited Right of Way Plan Development;  
Level 1 Bridge Design;

ENVIRONMENTAL SERVICES:

Waterway Permits;

**Selection Subfactors**

Past Experience in working with railroads, particularly Norfolk Southern Railroad.

**Estimated Date of Authorization**

It is anticipated that the selected Consultant will be authorized to proceed by March 2019.

**Completion Schedule**

The plans are to be completed and on file with the City of Oregon within nine (9) months from the date of authorization.

**Suspended or Debarred Firms**

Firms included on the current Federal list of firms suspended or debarred are not eligible for selection.

**Compliance with Title VI of the Civil Rights Act of 1964**

The City of Oregon, in accordance with Title VI of the Civil Rights Act of 1964 (78 Stat. 252, 42 U.S.C. §§ 2000d to 2000d-4) and the Regulations, hereby notifies all bidders that it will affirmatively ensure that any contract entered into pursuant to this advertisement, all bidders including disadvantaged business enterprises will be afforded full and fair opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, national origin, sex, age, disability, low-income status, or limited English proficiency in consideration for an award.

## **Selection Procedures**

The LPA will directly select a consultant based on the Letter of Interest (Lol). The requirements for the Lol and the Programmatic Consultant Selection Rating Form that will be used to select the consultant are shown below.

Firms interested in being considered for selection should respond by submitting three (3) copies of the Letter of Interest and one (1) electronic pdf file of the entire qualification package to the following address **by 4:30 PM on the response due date** listed above.

**Paul Roman, P.E.  
Director of Public Service – City of Oregon  
5330 Seaman Road  
Oregon, Ohio 43616**

Responses received after 4:30 PM on the response due date will not be considered.

## **Scope of Services**

The Scope of Services document is included below.

## **Requirements for Letters of Interest, Programmatic Selection Process**

- A. Instructions for Preparing and Submitting a Letter of Interest
1. Provide the information requested in the Letter of Interest Content (Item B below), in the same order listed, in a letter signed by an officer of the firm. Do not send additional forms, resumes, brochures, or other material.
  2. Letters of Interest shall be limited to ten (10) 8½" x 11" single sided pages plus two (2) pages for the Project Approach (Item B.5 below).
  3. Please adhere to the following requirements in preparing and binding letters of interest:
    - a. Please use a minimum font size of 12-point and maintain margins of 1" on all four sides.
    - b. Page numbers must be centered at the bottom of each page.
    - c. Use 8½" x 11" paper only.
    - d. Bind letters of interest by stapling at the upper left hand corner only. Do not utilize any other binding system.
    - e. Do not provide tabbed inserts or other features that may interfere with machine copying.
- B. Letter of Interest Content
1. A statement of the firm's qualifications to provide the requested services. Describe the expertise and experience of your firm in providing the proposed services on projects of similar size. Identify and describe three (3) projects which your firm has completed over the past five (5) years that have similarities and relevance to this proposed project and scope of services. Provide a list of at least five (5)

references, including name, address and telephone number of previous clients on projects with a similar scope of work. List the types of services for which your firm is currently prequalified by the Ohio Department of Transportation.

2. List significant subconsultants, their current prequalification categories and the percentage of work to be performed by each subconsultant.
3. List the Project Manager and other key staff members, including key subconsultant staff. Include project engineers for important disciplines and staff members that will be responsible for the work, and the project responsibility of each.

Address the experience of the key staff members on similar projects, and the staff qualifications relative to the selection subfactors noted.

4. Describe the capacity of your staff and their ability to perform the work in a timely manner, relative to present workload, and the availability of the assigned staff.
5. Provide a description of your Project Approach, not to exceed two pages. Confirm that the firm has visited the site and address your firm's: 1) Technical approach; 2) Understanding of the project; 3) Qualifications for the project; 4) Knowledge and experience concerning relevant ODOT and local standards, procedures and guidance documents; 5) Innovative ideas; 6) Project specific plan for ensuring increased quality, reduced project delivery time and reduced project costs.

Items 1 thru 4 must be included within the 10-page body of the Lol. Remaining space within the ten (10) pages may be utilized to provide personnel resumes or additional information concerning general qualifications.

Consultant Selection Rating Form  
for  
Programmatic Selections

Project: **LUC- York Street Bridge &  
Railroad Bridge over Amolsch Ditch**  
PID: **N/A**  
Project Type: **Bridge**

Selection Committee Members:  
**Dir. of Public Service - Oregon**  
**Deputy City Engineer - Oregon**  
**Staff Engineer - Oregon**

Firm Name: \_\_\_\_\_

Category	Total Value	Scoring Criteria	Score
<b>Management &amp; Team</b>			
Project Manager	10	See Note 1, Exhibit 1	
Strength/Experience of Assigned Staff including Subconsultants	25	See Note 2, Exhibit 1	
Firm's Current Workload/ Availability of Personnel	10	See Note 4, Exhibit 1	
<b>Consultant's Past Performance</b>	30	See Note 3, Exhibit 1	
<b>Project Approach</b>	25		
<b>Total</b>	100		

If Applicable: Adequate good faith efforts made to meet DBE goal            Y/N

**Exhibit 1 - Consultant Selection Rating Form Notes**

1. The proposed project manager for each consultant shall be ranked, with the highest ranked project manager receiving the greatest number of points, and lower ranked project managers receiving commensurately lower scores. The rankings and scores should be based on each project manager's experience on similar projects and past performance for the LPA and other agencies. The selection committee may contact ODOT and outside agencies if necessary. Any subfactors identified should be weighed heavily in the differential scoring.

Differential scoring should consider the relative importance of the project manager's role in the success of a given project. The project manager's role in a simple project may be less important than for a complex project, and differential scoring should reflect this, with higher differentials assigned to projects that require a larger role for the project manager.

2. The experience and strength of the assigned staff, including subconsultant staff, should be ranked and scored as noted for Number 1 above, with higher differential scores assigned on more difficult projects. Any subfactors identified in the project notification should be weighed heavily in the differential scoring.

As above, other agencies may be contacted.

3. The consultants' past performance on similar projects shall be ranked and scored on a relative, differential scoring type basis, with the highest ranked consultant receiving a commensurately greater number of points. The selection team should consider ODOT CES performance ratings if available, and consult other agencies as appropriate. The use of CES ratings shall place emphasis on the specific type of services requested.

The differential scoring should consider the complexity of the project and any subfactors identified in the project notification.

4. The consultant's workload and availability of qualified personnel, equipment and facilities shall be ranked and scored on a relative, differential scoring type basis. The scoring shall consider quantifiable concerns regarding the ability of a firm (or firms) rated higher in other categories to complete the work with staff members named in the letter of interest.

## **Scope of Services**

The Professional Consulting Firm (Engineer) will be responsible for preparing plans and specifications for the complete replacement of the York Street Bridge over Amolsch Ditch (SFN 4863232) and the Norfolk Southern Railroad Bridge over Amolsch Ditch.

The following list outlines the Firm's Scope of Work. These requirements include, but are not limited to the following professional services:

### Preliminary Engineering

1. Field Survey and Base Mapping
2. Geotechnical Investigation
3. Drainage
4. Hydraulic Analysis
5. Railroad Coordination and Permitting
6. Bridge Type Study
7. Cost Comparisons
8. Detour Plan
9. Utility Investigation

### Design Development, Roadway

1. Title Sheet
2. General Notes
3. Typical Sections
4. Plan and Profiles

5. Cross Sections
6. Detour Plan and MOT Notes
7. Signing and Striping
8. Drainage
9. BMP
10. SWPPP
11. Utility Identification and Coordination

#### Design Development, Structure

1. Site Plan
2. General Notes
3. Removal Plan
4. Foundation Design and Plan
5. Substructure and Wingwall Design
6. Superstructure Design
7. Reinforcing Schedule
8. Load Rating

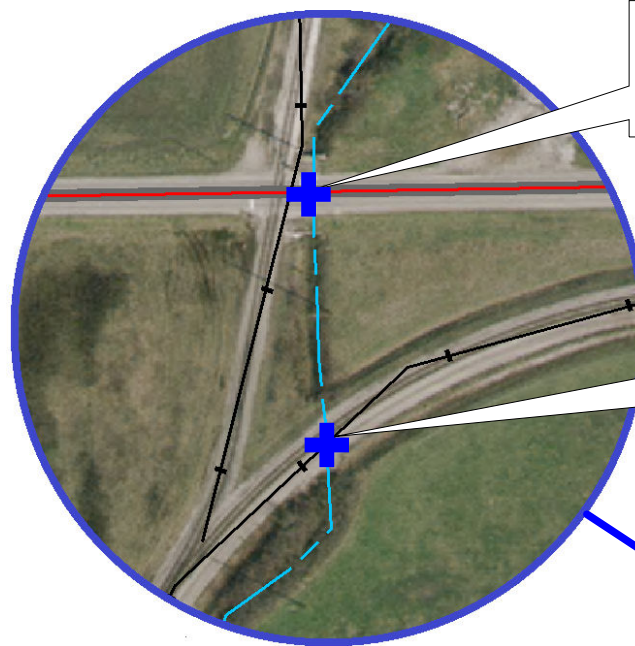
#### Right of Way Plan

1. Research
2. Control Points
3. Submittal of Right of Way Plans
4. Legal Descriptions for Easements (If necessary)

#### Miscellaneous Items

1. Submittals to the City of Oregon and Norfolk Southern Railroad
2. Project Management and Plan Review
3. Opinion of Probable Costs (Updated at Each Review Submittal)
4. Utility Submittals

**CEDAR POINT RD**



York Street Bridge  
Over Otter Creek  
Twin 54" RCPs

NS RR Bridge  
Over Otter Creek  
Twin 54" CMPs

**OTTER CREEK RD**

**N LALLENDORF RD**

**YORK ST**

Project  
Location

Norfolk Southern Railroad

AMLOSCH-DRIFTMEY

DRIFTMEYER DITCH

OTTER CREEK

**CORDUROY RD**

